

HISTORIC AND DESIGN REVIEW COMMISSION

May 17, 2023

HDRC CASE NO: 2022-428
ADDRESS: 1614 E HOUSTON ST
LEGAL DESCRIPTION: NCB 577 BLK 16 LOT 5
ZONING: RM-4, H
CITY COUNCIL DIST.: 2
DISTRICT: Dignowity Hill Historic District
APPLICANT: William Brewer/Done Right Construction DBA
OWNER: GOFORTH DERETHA & PATRICIA SCOTT
TYPE OF WORK: New construction of a 1.5-story, single-family structure
APPLICATION RECEIVED: May 08, 2023
60-DAY REVIEW: Not applicable due to City Council Emergency Orders
CASE MANAGER: Claudia Espinosa

REQUEST:

The applicant is requesting a Certificate of Appropriateness for approval to: Construct a 1.5-story, single-family structure.

APPLICABLE CITATIONS:

Historic Design Guidelines, Chapter 4, New Construction

1. Building and Entrance Orientation

A. FAÇADE ORIENTATION

- i. *Setbacks*—Align front facades of new buildings with front facades of adjacent buildings where a consistent setback has been established along the street frontage. Use the median setback of buildings along the street frontage where a variety of setbacks exist. Refer to UDC Article 3, Division 2. Base Zoning Districts for applicable setback requirements.
- ii. *Orientation*—Orient the front façade of new buildings to be consistent with the predominant orientation of historic buildings along the street frontage.

B. ENTRANCES

- i. *Orientation*—Orient primary building entrances, porches, and landings to be consistent with those historically found along the street frontage. Typically, historic building entrances are oriented towards the primary street.

2. Building Massing and Form

A. SCALE AND MASS

- i. *Similar height and scale*—Design new construction so that its height and overall scale are consistent with nearby historic buildings. In residential districts, the height and scale of new construction should not exceed that of the majority of historic buildings by more than one-story. In commercial districts, building height shall conform to the established pattern. If there is no more than a 50% variation in the scale of buildings on the adjacent block faces, then the height of the new building shall not exceed the tallest building on the adjacent block face by more than 10%.
- ii. *Transitions*—Utilize step-downs in building height, wall-plane offsets, and other variations in building massing to provide a visual transition when the height of new construction exceeds that of adjacent historic buildings by more than one-half story.
- iii. *Foundation and floor heights*—Align foundation and floor-to-floor heights (including porches and balconies) within one foot of floor-to-floor heights on adjacent historic structures.

B. ROOF FORM

- i. *Similar roof forms*—Incorporate roof forms—pitch, overhangs, and orientation—that are consistent with those predominantly found on the block. Roof forms on residential building types are typically sloped, while roof forms on non-residential building types are more typically flat and screened by an ornamental parapet wall.

C. RELATIONSHIP OF SOLIDS TO VOIDS

i. *Window and door openings*—Incorporate window and door openings with a similar proportion of wall to window space as typical with nearby historic facades. Windows, doors, porches, entryways, dormers, bays, and pediments shall be considered similar if they are no larger than 25% in size and vary no more than 10% in height to width ratio from adjacent historic facades.

ii. *Façade configuration*—The primary façade of new commercial buildings should be in keeping with established patterns. Maintaining horizontal elements within adjacent cap, middle, and base precedents will establish a consistent street wall through the alignment of horizontal parts. Avoid blank walls, particularly on elevations visible from the street. No new façade should exceed 40 linear feet without being penetrated by windows, entryways, or other defined bays.

D. LOT COVERAGE

i. *Building to lot ratio*—New construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. Limit the building footprint for new construction to no more than 50 percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building to lot ratio.

3. Materials and Textures

A. NEW MATERIALS

i. *Complementary materials*—Use materials that complement the type, color, and texture of materials traditionally found in the district. Materials should not be so dissimilar as to distract from the historic interpretation of the district. For example, corrugated metal siding would not be appropriate for a new structure in a district comprised of homes with wood siding.

ii. *Alternative use of traditional materials*—Consider using traditional materials, such as wood siding, in a new way to provide visual interest in new construction while still ensuring compatibility.

iii. *Roof materials*—Select roof materials that are similar in terms of form, color, and texture to traditionally used in the district.

iv. *Metal roofs*—Construct new metal roofs in a similar fashion as historic metal roofs. Refer to the Guidelines for Alterations and Maintenance section for additional specifications regarding metal roofs.

v. *Imitation or synthetic materials*—Do not use vinyl siding, plastic, or corrugated metal sheeting. Contemporary materials not traditionally used in the district, such as brick or simulated stone veneer and Hardie Board or other fiberboard siding, may be appropriate for new construction in some locations as long as new materials are visually similar to the traditional material in dimension, finish, and texture. EIFS is not recommended as a substitute for actual stucco.

B. REUSE OF HISTORIC MATERIALS

Salvaged materials—Incorporate salvaged historic materials where possible within the context of the overall design of the new structure.

4. Architectural Details

A. GENERAL

i. *Historic context*—Design new buildings to reflect their time while respecting the historic context. While new construction should not attempt to mirror or replicate historic features, new structures should not be so dissimilar as to distract from or diminish the historic interpretation of the district.

ii. *Architectural details*—Incorporate architectural details that are in keeping with the predominant architectural style along the block face or within the district when one exists. Details should be simple in design and should complement, but not visually compete with, the character of the adjacent historic structures or other historic structures within the district. Architectural details that are more ornate or elaborate than those found within the district are inappropriate.

iii. *Contemporary interpretations*—Consider integrating contemporary interpretations of traditional designs and details for new construction. Use of contemporary window moldings and door surroundings, for example, can provide visual interest while helping to convey the fact that the structure is new. Modern materials should be implemented in a way that does not distract from the historic structure.

5. Garages and Outbuildings

A. DESIGN AND CHARACTER

i. *Massing and form*—Design new garages and outbuildings to be visually subordinate to the principal historic structure in terms of their height, massing, and form.

ii. *Building size*—New outbuildings should be no larger in plan than 40 percent of the principal historic structure footprint.

iii. *Character*—Relate new garages and outbuildings to the period of construction of the principal building on the lot through the use of complementary materials and simplified architectural details.

- iv. *Windows and doors*—Design window and door openings to be similar to those found on historic garages or outbuildings in the district or on the principle historic structure in terms of their spacing and proportions.
- v. *Garage doors*—Incorporate garage doors with similar proportions and materials as those traditionally found in the district.

B. SETBACKS AND ORIENTATION

- i. *Orientation*—Match the predominant garage orientation found along the block. Do not introduce front-loaded garages or garages attached to the primary structure on blocks where rear or alley-loaded garages were historically used.
- ii. *Setbacks*—Follow historic setback pattern of similar structures along the streetscape or district for new garages and outbuildings. Historic garages and outbuildings are most typically located at the rear of the lot, behind the principal building. In some instances, historic setbacks are not consistent with UDC requirements and a variance may be required.

6. Mechanical Equipment and Roof Appurtenances

A. LOCATION AND SITING

- i. *Visibility*—Do not locate utility boxes, air conditioners, rooftop mechanical equipment, skylights, satellite dishes, and other roof appurtenances on primary facades, front-facing roof slopes, in front yards, or in other locations that are clearly visible from the public right-of-way.
- ii. *Service Areas*—Locate service areas towards the rear of the site to minimize visibility from the public right-of-way.

B. SCREENING

- i. *Building-mounted equipment*—Paint devices mounted on secondary facades and other exposed hardware, frames, and piping to match the color scheme of the primary structure or screen them with landscaping.
- ii. *Freestanding equipment*—Screen service areas, air conditioning units, and other mechanical equipment from public view using a fence, hedge, or other enclosure.
- iii. *Roof-mounted equipment*—Screen and set back devices mounted on the roof to avoid view from public right-of-way.

7. Designing for Energy Efficiency

A. BUILDING DESIGN

- i. *Energy efficiency*—Design additions and new construction to maximize energy efficiency.
- ii. *Materials*—Utilize green building materials, such as recycled, locally-sourced, and low maintenance materials whenever possible.
- iii. *Building elements*—Incorporate building features that allow for natural environmental control – such as operable windows for cross ventilation.
- iv. *Roof slopes*—Orient roof slopes to maximize solar access for the installation of future solar collectors where compatible with typical roof slopes and orientations found in the surrounding historic district.

B. SITE DESIGN

- i. *Building orientation*—Orient new buildings and additions with consideration for solar and wind exposure in all seasons to the extent possible within the context of the surrounding district.
- ii. *Solar access*—Avoid or minimize the impact of new construction on solar access for adjoining properties.

C. SOLAR COLLECTORS

- i. *Location*—Locate solar collectors on side or rear roof pitch of the primary historic structure to the maximum extent feasible to minimize visibility from the public right-of-way while maximizing solar access. Alternatively, locate solar collectors on a garage or outbuilding or consider a ground-mount system where solar access to the primary structure is limited.
- ii. *Mounting (sloped roof surfaces)*—Mount solar collectors flush with the surface of a sloped roof. Select collectors that are similar in color to the roof surface to reduce visibility.
- iii. *Mounting (flat roof surfaces)*—Mount solar collectors flush with the surface of a flat roof to the maximum extent feasible. Where solar access limitations preclude a flush mount, locate panels towards the rear of the roof where visibility from the public right-of-way will be minimized.

8. Medium-Density and Multifamily

A. SITE SELECTION & DEVELOPMENT

- i. *Location & Context* – The size, depth, and accessibility of lots varies from district to district, and block to block. Regardless of allowable density by zoning, the existing development pattern will inform what building forms and sizes are achievable under the Historic Design Guidelines. Consider lots that historically featured higher density or commercial uses as opportunities for multifamily infill, or lots that allow for the addition of larger building forms or groupings away from the public realm.

ii. *Building Separation & Groupings* – Incorporate multiple dwelling units into historically-common building sizes and forms within the established context area. For example, in context areas having larger buildings, four units may be appropriately combined into a single, two-story building form. In context areas with smaller buildings, a more appropriate response would be to separate the units into smaller, individual building forms.

iii. *Preservation of Open Space* – As multiple buildings are proposed for a site, they should be separated and scaled in a manner that preserves open space consistent with the established context area. For example, if the context area predominately consists of a primary structure separated from a rear accessory structure by a common distance, then the proposed development should follow a similar pattern. Preserved open space may be used for common areas, amenity space, or uncovered parking.

B. FACADE ORIENTATION & ENTRANCES

i. *Setbacks*—Align front facades of new buildings with front facades of adjacent buildings where a consistent setback has been established along the street frontage. Use the median front setback of buildings within the established context area where a variety of setbacks exist.

ii. *Orientation*—Orient the front façade of new buildings to be consistent with the predominant orientation of historic buildings along the street frontage. Street-facing facades that are void of fenestration or a street-facing entrance are strongly discouraged.

C. SCALE, MASSING, AND FORM

i. *Building footprint* - new construction should be consistent with adjacent historic buildings in terms of the building to lot ratio. Using the established context area as reference, limit the total building footprint for new construction to no more than 50 percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building to lot ratio. Similarly, individual building footprints should not exceed the average building footprint of primary structures in the established context area by more than 50%.

ii. *Impervious Cover* – In addition to building footprints, other areas of impervious lot coverage (such as parking pads or driveways) should be minimized. Developments with building footprints that meet or exceed 50% of the total lot area should utilize pervious and semi-pervious paving materials and stormwater retention strategies wherever possible.

iii. *Building Height*—Design new construction so that its height and overall scale are consistent with historic buildings in the established context area. In residential districts, the overall height of new construction should not exceed the height of adjacent or nearby historic buildings by more than 50% when measured from similar elevation points such as the ground plane and the highest ridge line of the roof regardless of roof pitch or form. Buildings that exceed the height of immediately adjacent historic buildings by any amount should utilize the following strategies:

(a). *Half Stories* - Incorporating additional height into half stories or fully within traditional sloped roof forms is strongly encouraged.

(b). *Transitions* - Utilize step-downs in building height, wall-plane offsets, and other variations in building massing to provide a visual transition to the neighboring properties.

(c). *Roof Form* – Utilize roof forms that reduce visual prominence when viewed from the street such as hip, side gable, or hip-on-gable (jerkinhead).

iv. *Traditional Forms and Spatial Relationships* – In residential districts, there is often an established pattern of a larger, primary structure facing the street with smaller, accessory structures located at the rear of the property. Design and site new buildings to be consistent with this development pattern where evident within the established context area.

v. *Foundation and Floor Heights*—Align foundation and floor-to-floor heights (including porches and balconies) within one foot of floor-to-floor heights on historic buildings within the established context area.

D. ARCHITECTURAL FORMS

i. *Primary Roof Forms* - Incorporate roof forms—pitch, overhangs, and orientation—that are consistent with those found in the established context area. Flat or shed roofs are not typical of primary structures in San Antonio's residential historic districts and should be avoided.

ii. *Porches* – Utilize traditional front porch depths and forms to establish a pedestrian scale along the street frontage. Porch designs should be similar in dimension and form as those found on historic buildings within the established context area.

iii. *Bays* – Separate building massing into distinguishable architectural bays consistent with historic buildings within the established context area. This is best accomplished through a change in wall plane or materials, or by aligning appropriately-scaled fenestrations.

E. RELATIONSHIP OF SOLIDS TO VOIDS

i. *Window and door openings*—Incorporate window and door openings with a similar proportion of wall to window space as found within the established context area. Windows, doors, porches, entryways, dormers, bays, and pediments shall be considered similar if they are no larger than 25% in size and vary no more than 10% in height to width ratio from adjacent historic facades.

ii. *Window Specifications* – All windows used in new construction should adhere to adopted guidelines and policy for windows in terms of type, materials, proportions, profile, and installation details. A summary is provided on this page for reference.

F. PARKING AND ACCESS

i. *Location* – Site parking areas centrally within a development or to one side of the proposed structures. Limiting on-site parking to the traditional front yard space is strongly discouraged.

ii. *Parking Surfaces & Design* – Pervious or semipervious surfaces are strongly encouraged. Incorporate parking opportunities into a comprehensive landscaping and hardscaping plan that is consistent with the Historic Design Guidelines.

iii. *Garages* - Attached garages, especially front-loading garages, are strongly discouraged. Detached garages designed to be consistent with this chapter may be considered where lot coverage allows. Uncovered surface parking is encouraged when the recommended building-to-lot ratio has been exceeded.

iv. *Driveways and Curb Cuts* – A single, 10-foot driveway at one street frontage is recommended. Projects should first attempt to utilize historic curb cuts where extant. Additional entry points may be considered where there is alley access. The addition of driveways should not confuse or alter the historic development pattern. Do not introduce wide, shared driveways that appear visually similar to a street.

Standard Specifications for Windows in New Construction

- GENERAL: New windows on additions should relate to the windows of the primary historic structure in terms of materiality and overall appearance. Windows used in new construction should be similar in appearance to those commonly found within the district in terms of size, profile, and configuration. While no material is expressly prohibited by the Historic Design Guidelines, a high-quality wood or aluminum-clad wood window product often meets the Guidelines with the stipulations listed below. Whole window systems should match the size of historic windows on property unless otherwise approved.
- SIZE: Windows should feature traditional dimensions and proportions as found within the district.
- SASH: Meeting rails must be no taller than 1.25". Stiles must be no wider than 2.25". Top and bottom sashes must be equal in size unless otherwise approved.
- DEPTH: There should be a minimum of 2" in depth between the front face of the window trim and the front face of the top window sash.
- This must be accomplished by recessing the window sufficiently within the opening or with the installation of additional window trim to add thickness.
- TRIM: Window trim must feature traditional dimensions and architecturally appropriate casing and sloped sill detail. Window track components such as jamb liners must be painted to match the window trim or concealed by a wood window screen set within the opening.
- GLAZING: Windows should feature clear glass. Low-e or reflective coatings are not recommended for replacements. The glazing should not feature faux divided lights with an interior grille. If approved to match a historic window configuration, the window should feature real exterior muntins.
- COLOR: Wood windows should feature a painted finished. If a clad product is approved, white or metallic manufacturer's color is not allowed, and color selection must be presented to staff.

FINDINGS:

- a. The primary historic structure located at 1614 E Houston is a single-family, residential structure constructed circa 1928 in the Craftsman style. The structure first appears on the 1931 Sanborn Map. The historic structure features a low-pitched, composition shingle roof with side gables, wood siding, an asymmetrical front porch with wrought iron columns, and one-over-one wood windows. The rear of the property features an accessory structure with a low-pitched, composite shingle roof, wood siding, a wood garage door, and a wood entry door. The property is contributing to the Dignowity Hill Historic District. At this time, the applicant is requesting final approval to construct a new single-family structure to replace the existing structures on the lot.
- b. CASE HISTORY – On May 3, 2023, the HDRC approved the request for conceptual approval for new construction of a 1.5-story single-family structure. The applicant has provided a salvage plan, replacement plans, and a plan that shows the salvaged architectural elements. The applicant has submitted replacement plans for final approval. The plans meet the HDRC stipulation for the reconstruction of the front façade.

- c. **GENERAL DESIGN** – As stipulated at the time of demolition approval, the applicant must reconstruct the original façade. The original house featured a low, cross-gabled roof with inset front porch and side entry. The proposed design includes an inset porch at a similar location and a reconstruction of the original design as stipulated. The proposed roof form is compatible with the original structure. Staff further finds that a side-facing entry is appropriate and is consistent with the reconstruction of the front façade as stipulated by the HDRC Commission.
- d. **SETBACKS & ORIENTATION (HOUSTON)** – According to the Guidelines for New Construction, the front facades of new buildings should align with the front facades of adjacent buildings where a consistent setback has been established along the street frontage. Additionally, the orientation of new construction should be consistent with the historic examples found on the block. The applicant has indicated that the structure will be in the same location as the previous structure, which is consistent with the adjacent historic structure located to the east. The applicant has expressed that the adjacent properties feature a setback of 24.7' as shown in the provided site plan. At this time, the applicant has provided a detailed site plan that shows the location of the neighboring structure. Staff finds that the proposed front setback is generally appropriate. The applicant has modified the setback on the west side of the property to be consistent with the neighboring setback; however, the neighboring setback will remove the portion of the porch that wraps around the east façade.
- e. **SCALE & MASS** – The applicant has proposed to construct an approximately 1,056 square-foot 1.5-story residential structure. According to Guideline 2.A.i for New Construction, new structures should feature a height and massing that is similar to historic structures in the vicinity. In residential districts, the height and scale of new construction should not exceed that of the majority of historic buildings by more than one story. This block within the Dignowity Hill Historic District features 1-story historic structures. Staff finds that the proposed scale and massing of the structure appears generally appropriate.
- f. **PORCH RECONSTRUCTION** – The original front porch form featured a side entryway, two sashed windows, and four wrought iron columns. Additionally, the existing front porch extends beyond the front façade projection and wraps beyond the front façade to the east. The applicant has proposed to reconstruct the porch to include the side entryway, two sashed windows, and four porch columns. Staff finds the proposed front porch to be consistent with the reconstruction of the front porch as required by the HDRC.
- g. **DRIVEWAY** – The applicant is requesting to extend the existing driveway apron to 16.4 feet in width and continue the driveway to the rear of the structure. Staff finds this request to be inconsistent with Guidelines for Site Elements 5.B.i., to retain and repair in place historic driveway configurations, such as ribbon drives. Incorporate a similar driveway configuration—materials, width, and design—to that historically found on the site. Additionally, staff finds that the proposed driveway be separated from the proposed new construction by a landscaping strip, and be no more than ten (10) feet in width.
- h. **ROOF (FORM)** – The applicant has proposed a hipped roof form with a dormer for the proposed new construction. According to Guideline 2.B.i for New Construction, new construction should feature roof forms that are consistent with those predominantly found on the block. Staff finds this request is reflective of the roof forms found in the Dignowity Hill Historic District and is consistent with the guidelines.
- i. **LOT COVERAGE** – Guideline 2.D.i for New Construction stipulates that building-to-lot ratio for new construction should be consistent with adjacent historic buildings. Limit the building footprint for new construction to no more than 50 percent of the total lot area, unless adjacent historic buildings establish a precedent with a greater building-to-lot ratio. Staff finds the proposal consistent with the Guidelines, per the submitted site plan.
- j. **MATERIALS AND TEXTURES** – The applicant has proposed to reconstruct the front façade with salvaged materials from the deconstruction of the primary structure and construct the remaining of the residence with Hardie siding and a composition shingle roof. The applicant has also expressed that they would like to install salvaged materials from the original structure pending the deconstruction of the structure. Staff finds this request to be inconsistent with the guidelines for New Construction 3.A.i use materials that complement the type, color, and texture of materials traditionally found in the district. Materials should not be so dissimilar as to distract from the historic interpretation of the district. The materials on the existing structure is 117 waterfall siding, salvaged materials and selected matching wood siding must be coordinated with staff. **WINDOW MATERIALS** – The applicant has expressed that they would like to install salvaged windows from the deconstructed structure. Additionally, the applicant will likely need to introduce matching, new wood windows. Staff finds that any new windows should be fully wood and feature an inset of two (2) inches within facades and should feature profiles that are found historically within the immediate vicinity. Window trim must feature traditional dimensions and an architecturally appropriate sill detail. Window track components must be painted to match the window trim or be concealed by a wood window screen set within the opening. Faux divided lites are not permitted. Staff finds that

all windows installed should feature traditional operations and that the applicant should submit product specifications for review prior to returning to the HDRC for final approval.

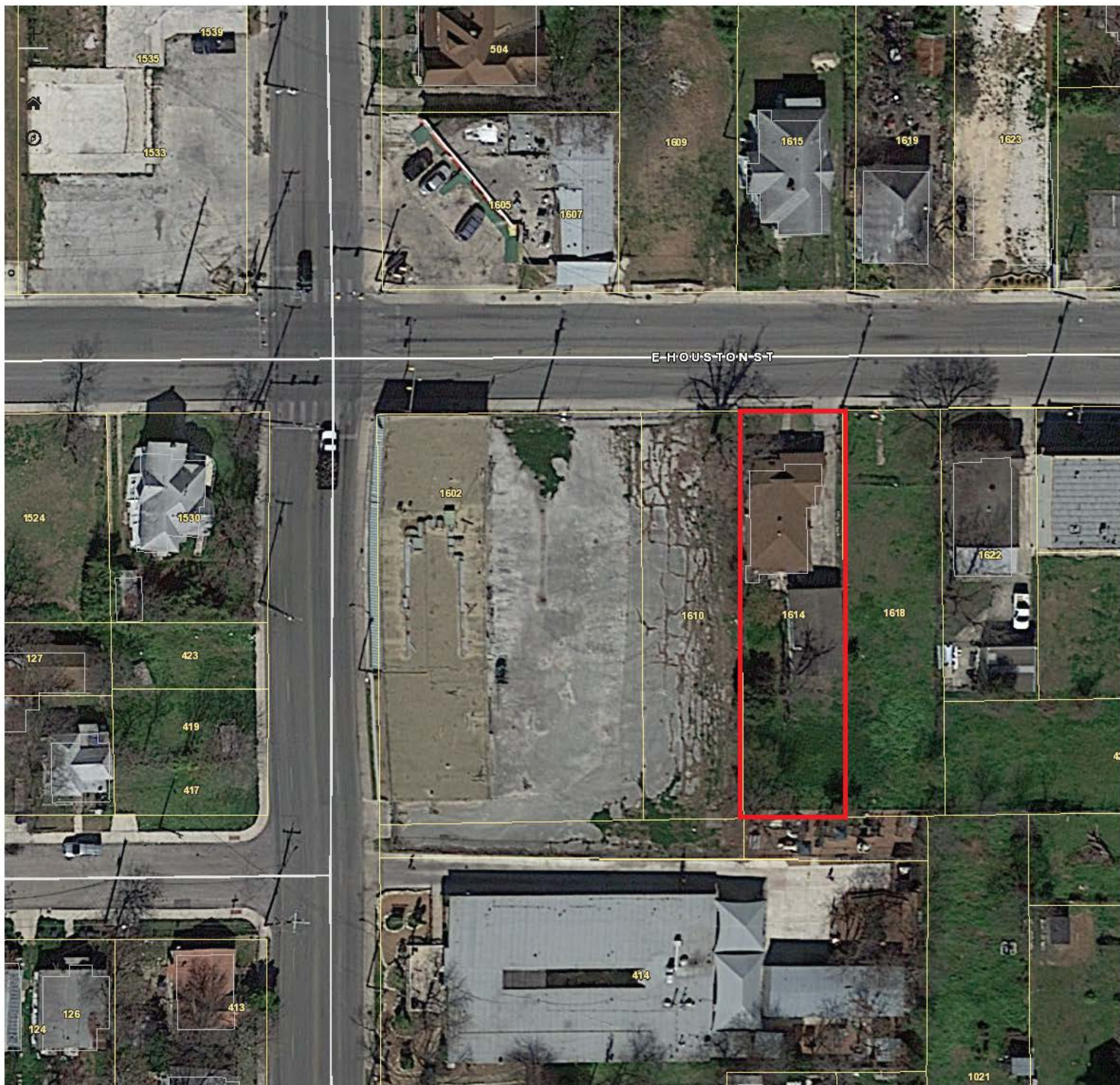
- k. **RELATIONSHIP OF SOLIDS TO VOIDS** – Guideline 2.C.i for New Construction stipulates that new construction should incorporate window and door openings with a similar proportion of wall-to-window space as typical with nearby historic facades. Windows, doors, porches, entryways, dormers, bays, and pediments shall be considered similar if they are no larger than 25% in size and vary no more than 10% in height-to-width ratio from adjacent historic facades. The applicant has proposed a fenestration pattern on the front façade that features a side entryway and double-hung windows that are reflective of the original structure, staff finds this to be appropriate. The fenestration pattern on the remaining elevation consists of double-hung windows, two rear doors, and a wooden entry door. Staff finds that the applicant’s proposed fenestration to feature traditional proportions and window configurations commonly found in the district.
- l. **ARCHITECTURAL DETAILS** – Guideline 4.A.i for New Construction states that new buildings should be designed to reflect their time while respecting the historic context. While new construction should not attempt to mirror or replicate historic features, new structures should not be so dissimilar as to distract from or diminish the historic interpretation of the district. Additionally, architectural details should be complementary in nature and should not detract from nearby historic structures. Generally, staff finds the proposed architectural details to be appropriate.
- m. **FENCING** – The applicant has proposed to install a four-foot-tall wrought iron fence in the front yard and a six-foot-tall wood rear privacy fence. Per the Fence Policy that was adopted in September 2021, rear yard privacy fences should be no taller than six feet in height and should feature wood construction. Front yard fences should match the height of neighboring fences or should be limited to four feet in height. Staff finds this request to be appropriate and eligible for Administrative Approval. A driveway gate is not shown in the drawings; more information about design and placement of the gate will need to be submitted for future consideration.
- n. **REAR DECK** – The applicant is requesting to install a rear deck measuring approximately 284.05 square feet. The applicant has not submitted materials for the rear deck. Generally, staff finds the size of the deck appropriate; however, this portion of the request is incomplete, and staff finds that the applicant should submit a separate Certificate of Appropriateness application for rear deck installation to staff for review.
- o. **MECHANICAL EQUIPMENT** – Per Guideline 6.B.ii for New Construction, all mechanical equipment should be screened from view at the public right-of-way.

RECOMMENDATION:

Staff recommends approval based on findings a through q, with the following stipulations:

- i. That any new windows be fully wood and include the specifications listed in finding l. Any new windows should be fully wood and feature an inset of two (2) inches within facades and should feature profiles that are found historically within the immediate vicinity. Window trim must feature traditional dimensions and an architecturally appropriate sill detail. Window track components must be painted to match the window trim or be concealed by a wood window screen set within the opening. Faux divided lites are not permitted. All windows installed should feature traditional operations and that the applicant should submit product specifications for review prior to returning to the HDRC for final approval. Accurate drawings must be submitted for verification by staff.
- ii. That the applicant submits an accurate site plan for the proposed rear deck for staff review prior to the issuance of a Certificate of Appropriateness based on finding p.
- iii. That the applicant meets all setback standards as required by city zoning requirements and obtains a variance from the Board of Adjustment if applicable.
- iv. That the proposed driveway be separated from the proposed new construction by a landscaping strip, and be no more than ten (10) feet in width. The width of the existing curb cut should be maintained. A new walkway from the sidewalk to the front porch should be installed to similar to what was previously on the site.
- v. Staff does not recommend approval of the proposed reconstruction with Hardie siding based on finding j. Staff recommends the applicant replace the siding with materials that match the existing siding in profile, dimensions, material, finish, and reveal.

A foundation inspection is to be scheduled with OHP staff to ensure that foundation setbacks and heights are consistent with the approved design. The inspection is to occur after the installation of form work and prior to the installation of foundation materials.



Residence
1614 E Houston St.
San Antonio, TX 78239

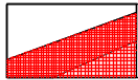
Structural Assessment – Fire Damage Report

Prepared by:

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New Braunfels, TX

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Date: July 5, 2022

Mrs. Dominique Miles
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Roof, Foundation and Framing Inspection, Fire Damage-1614 E. Houston St, San Antonio, TX.

OM Engineering was contracted to conduct a structural assessment for fire damage at the above property. The purpose of the inspection was to make visual observations on the extent of fire damage and to determine if the structure is salvageable. The assessment was conducted on July 1, 2022. The assessment did not include the verification of Insulation, Wind Bracing, Fire or Safety Code Compliance.

Select photographs from the inspection are presented at the end of this report, as well as a sketch with suggested repairs. Mr. William of Done Right Construction was on site.

The following information was provided via text messages and emails:

1. The fire was in 2019.
2. The property is an inheritance from Mary Manning.
3. The owner is Deretha Goforth.

A search of tax records revealed the following:

1. The residence was constructed in 1928.
2. The detached garage was constructed in 1980.

A visual inspection of the interior was conducted with the following observations:

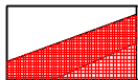
1. The rear walls and ceilings were severely damaged.
2. The damage consisted charring of the shiplap and wood walls studs.
3. The ceiling rafters and wood paneling was charred.
4. The fire damage extended from the rear into the front rooms.
5. The rear walls had collapsed, and the exterior rear wall was missing.
6. The floor deck was charred, buckled and missing sections.

A visual inspection of the exterior revealed the following observations:

1. The exterior cladding was aluminum siding and wood panels.
2. There were signs of damage to the aluminum siding along the east and north elevations.
3. The exterior framing was visibly racked to the west.
4. The roof line was visibly buckled and bowed.
5. The garage showed signs of weathering and age.
6. The garage roof framing was bowed with a hole in the roof deck.

A visual inspection of the attic revealed the following observations:

1. The roof framing consisted of rafter with purling bracing.
2. The rafters and purlin bracing were severely charred and damaged.
3. The rafters had collapsed at the rear and towards the front.
4. The roof deck was severely damaged and charred.



A visual inspection of the crawlspace revealed the following:

1. The foundation consisted of cedar post supporting wood beams and wood joists.
2. The cedar posts were toppled, tilted, or dislodged.
3. The wood beams were dislodged or tilted.
4. The wood joists were buckled and tilted.
5. The floor framing at the rear was severely damaged and charred.
6. An elevation survey was not conducted on the interior due to the conditions observed.

Discussion:

Fire damage to wood structures consists of smoke stains, desiccation of wood members, charring, failure of fasteners or complete destruction. The results to the structure can range from salvageable with minimal replacement to complete demolition. Based on the physical conditions observed, we approximate that 85% of the overall structure has been damaged from the fire and the structural integrity has been compromised.

The foundation consisted of cedar posts supporting wood beams and wood floor joists. The existing foundation piers were found to be toppled, tilted, or dislodged. Based on the condition of the overall structure, we can reasonably conclude that the foundation has also been compromised and unsalvageable.

Conclusion:

Based on the observations and the physical evidence, it is our opinion that the structure has experienced a severe fire and is structurally compromised. We believe that the structure is not salvageable and recommend the structure be demolished as soon as possible.

As discussed, onsite, we recommend mechanical methods for the demolition of the structure for safety concerns. The structure should be considered "unsafe" and under no conditions should anyone enter the interior or access the roof. The existing conditions for the detached garage also warrant demolition and the same methods should be considered.

Signature:

The opinions and findings expressed in this report are based upon the information available at the date of this report are the result of limited non-destructive visual investigation of the property and exposed building components. As such, OM Engineering, LLC assumes no liability for the misuse of this information by others and reserves the right to modify the conclusions contained herein upon receipt or discovery of additional information. Due to the limited access and the non-destructive nature of the investigation, OM Engineering, LLC cannot be held responsible for any hidden defects that may negatively impact the performance of the structure. This report is intended to provide an overview of the existing conditions and should not be used as an indicator of future performance; no expressed or implied warranties or guarantees of any kind are given. All sketches included are for illustrative purposes only.

We at OM Engineering, LLC sincerely thank you for the opportunity to serve you.

Sincerely,

Conan C. Bear, P.E.
Principal Engineer
OM Engineering, LLC
New Braunfels TX

Texas Firm No: 20120 Exp. 9-30-2022

1614 E Houston St, San Antonio, TX



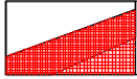


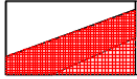
Photo Log:



Photo 1, North elevation, overall (front).



Photo 2, West elevation.



Photos 3, South (rear) elevation.



Photo 4, East elevation.

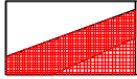


Photo 5, Exterior, damage to skirting and cladding, east elevation.



Photo 6, Interior, damage to framing and walls.

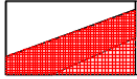


Photo 7, Interior, damage to walls and framing.



Photo 8, Attic, damaged roof framing.

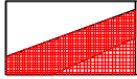


Photo 9, Crawlspace, toppled foundation piers.



Photo 10, Crawlspace, toppled piers and rolled wood beam.

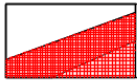


Photo 11, Crawlspace, damaged foundation pier.



Photo 12, Exterior, damaged roof, east elevation.

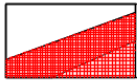


Photo 13, Exterior, garage roof.



Photo 14, Exterior, garage cladding.

OVERALL DAMAGE

Scale: 1/8" = 1'-0"

35'-0"



Structure Racked
in this direction

54'-0"

Fire Damage

Minimal fire damage
framing racked

Front
Door

DEMOLITION NOTES:

1. DO NOT DEMOLISH BY HAND.
2. WALL FRAMING STRUCTURAL INTEGRITY IS COMPROMISED.
3. ROOF FRAMING STRUCTURAL INTEGRITY IS COMPROMISED.
4. PIER & WOOD BEAM FOUNDATION IS COMPROMISED.
5. DEMO ROOF AND WALLS VIA MECHANICAL METHODS.
6. DO NOT ENTER STRUCTURE, CRAWLSPACE, OR CLIMB ON ROOF.

OBSERVED DAMAGES:

1. STRUCTURE HAS APPROXIMATELY 85% FIRE DAMAGE.
2. ROOF FRAMING IS COMPROMISED FROM FIRE DAMAGE.
3. INTERIOR WALLS (LOAD BEARING & NON-LOAD BEARING) ARE COMPROMISED FROM FIRE DAMAGE.
4. REAR EXTERIOR WALL IS MISSING, ADJACENT WALLS ARE COMPROMISED.
5. FRAMING IS RACKED TO THE WEST.
6. STRUCTURE IS PARTIALLY SUPPORTED ON THE CEDAR POSTS.
7. CEDAR FOUNDATION POSTS ARE TOPPLED, TILTED, OR DISLODGED.
8. FOUNDATION WOOD BEAMS ARE DAMAGED.

ELEVATION SURVEY LEGEND

- — — — — = Roof Lines
- - - - - = Extent of fire damage
= 3FT Door

NOTE:

1. DIMENSIONS ARE APPROXIMATE.
2. LOCATION OF INTERIOR WALLS ARE ESTIMATED.

The use of this drawing is limited at the property described in the title block. Any other use of this drawing is prohibited without the expressed written consent of OM Engineering LLC

OM Engineering LLC
Forensic Engineering

New Braunfels, TX
TX Firm No: 20120

316-768-0330
omengllc@gmail.com

Project:

1614 E Houston St.
San Antonio, TX 78239

Date:

7/1/2022

Revision:

0

Rev. Date:

XX/XX/XXXX

Drawing:

1/1

Done Right Construction

Estimate

William
5128064646
william@donerighthandyman.org

ESTIMATE NO.	020	ISSUE DATE	9/12/2022	VALID UNTIL	9/12/2022
FROM Done Right Construction 9607 marsh straw San Antonio TX 78354 United States		FOR Dominique Miles			
DESCRIPTION	QUANTITY	UNIT PRICE (\$)	AMOUNT (\$)		
Foundation repair and lift onto pier and beam 47/sqft for lift onto pier and beam 10/sqft for pier and beam pour Removal of old cedar piers \$1/sqft	1,252 sqft	58.00	72,616.00		
Roof removal and repair Removal of old shingles and install of new plywood and shingles	1,252 sqft	14.00	17,528.00		
Flooring removal and replacement \$1/sqft removal \$3.50/sqft replacement install&materials	1,252 sqft	4.50	5,634.00		
Walls and drywall Removal of fire damaged walls/ceilings and drywall Replace with new studs and drywall (tape/float/paint) Removal \$3/sqft Install \$21/sqft Paint \$2/swft	1,252 sqft	26.00	32,552.00		
Removal and replacement electrical Removal all aluminum wires and replace with current code approved wiring (romex 12/2) New sub panel and main panel New fixtures, outlets, switches	1,252 sqft	12.00	15,024.00		
Plumbing removal and replacement Replace all plumbing with pex piping	1,252 sqft	6.00	7,512.00		
Kitchen replacement Removal of fire damaged kitchen and install of new	1	12,000.00	12,000.00		
Removal of siding and install of new vinyl Removal all fire damaged siding and replace with new	1,252 sqft	6.00	7,512.00		
Sub floor replacement Removal all fire damaged floor plywood/wood and replace with new osb	1,252 sqft	2.00	2,504.00		
Window teplacement Remove and disposal of old windows Install and weather proofing of new windows	10 ea	850.00	8,500.00		

DESCRIPTION	QUANTITY	UNIT PRICE (\$)	AMOUNT (\$)
<div>Insulation</div> <div>Install of r13 batt insulation in walls and r31 in ceiling attic space.</div> <div>Removal and disposal of old insulation (asbestos removal) toxic removal</div>	1,252 sqft	14.00	17,528.00
<div>Toxic items removal fee</div> <div>House will need tent and special ppe for removal process. (Lead danger, asbestos danger)</div> <div>Dumpsters, dump fees</div>	1,252 sqft	15.00	18,780.00
<div>Fire damaged door replacement</div> <div>Replace doors damaged from fire</div>	5 ea	450.00	2,250.00
<div>Exterior repaint</div>	1,252 sqft	3.00	3,756.00
<div>Beam replacement</div> <div>Replace cedar beams</div>	11 each	2,800.00	30,800.00
Total (USD):			\$254,496.00

Demolition and Salvage Plan

Location: 1614 E Houston Street, San Antonio Texas 78202

Structure Type: Residential Pier & Beam (Wood Framing)

Method of Demolition: Deconstruction with Hydraulic shoring methods

Salvageable Materials: T&G siding, cedar piers, windows and frames, doors, T&G flooring, cedar beams, all other wood and material not deemed dangerous or fire damaged.

Property will be deconstructed following city code and safety guidelines; building will be shored up with hydraulic machinery for worker safety as we salvage materials for use in future buildings. Permitter fence will be put up to keep worksite and materials safe. All salvaged materials will be stored in storage until new building begins.

Oct 12, 2022 at 5:32:02 PM
1614 E Houston St
San Antonio TX 78202
United States



Oct 12, 2022 at 5:32:06 PM
1614 E Houston St
San Antonio TX 78202
United States

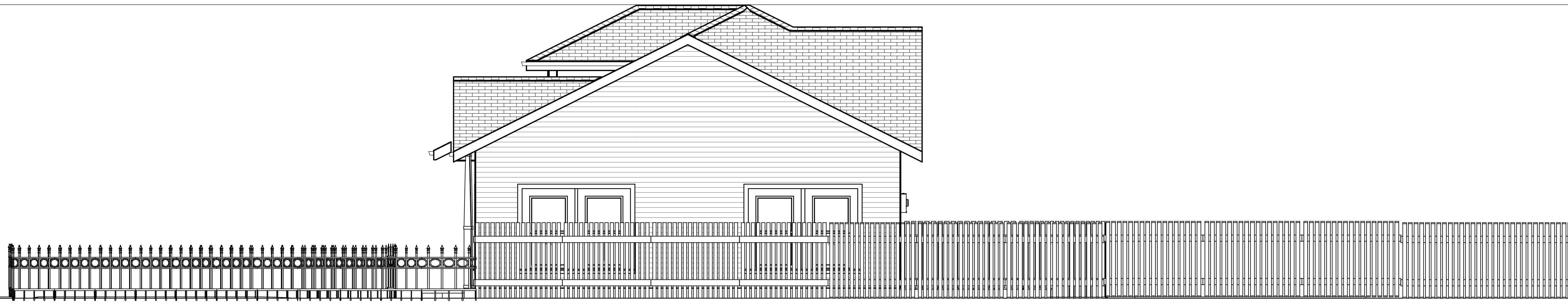


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San Antonio TX 78202
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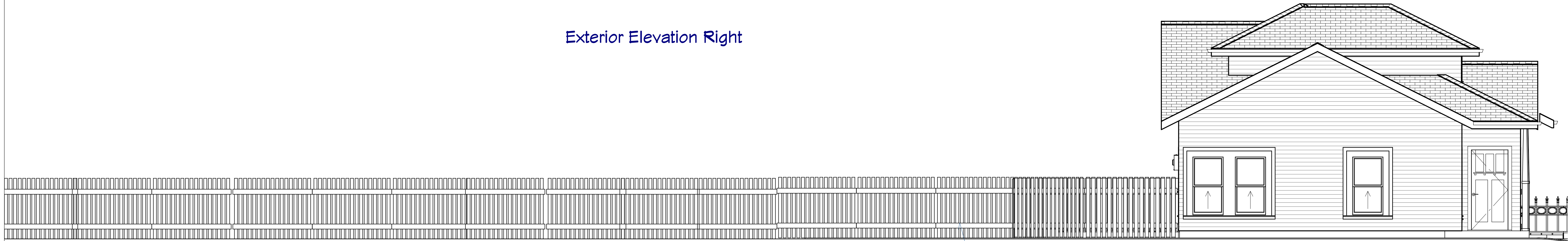


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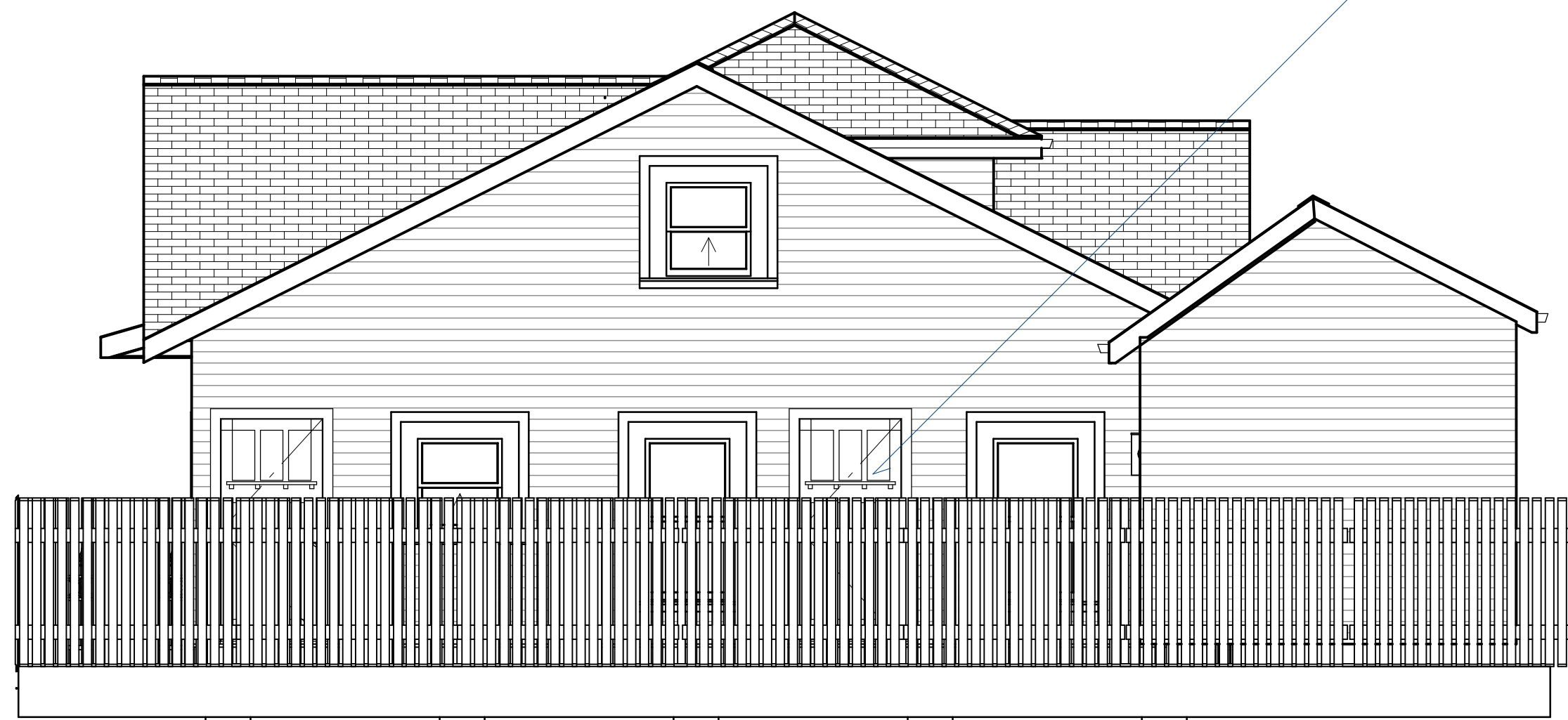




Exterior Elevation Right



Exterior Elevation Left



Exterior Elevation Back

ADDED 4 6" POSTS



Exterior Elevation Front

4 SHURBS IN FRONT OF WINDOWS

5 FT SETBACK ON SIDE OF HOME

REVISION TABLE	
NUMBER	DATE

REVISION

GOFORTH RESIDENCE
1614 E HOUSTON
SAN ANTONIO, TEXAS
78202

DRAWINGS PROVIDED BY:

DATE:

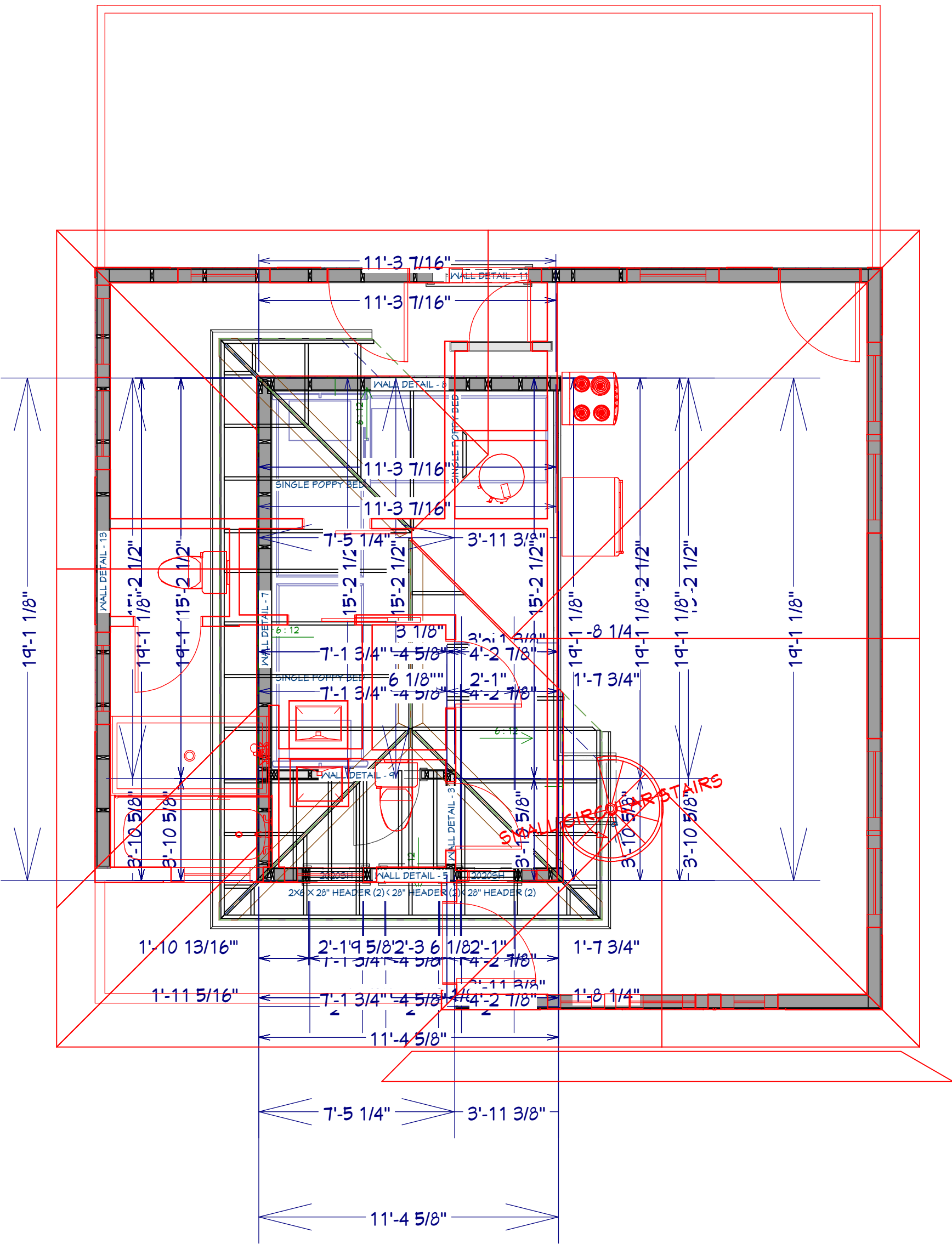
5/3/23

SCALE:

SHEET:

P-1





2nd Floor

REVISION TABLE		REVISION BY	DESCRIPTION
NUMBER	DATE		

REVISION

GOFORTH RESIDENCE
1614 E HOUSTON
SAN ANTONIO, TEXAS
78202

DRAWINGS PROVIDED BY:

DATE:

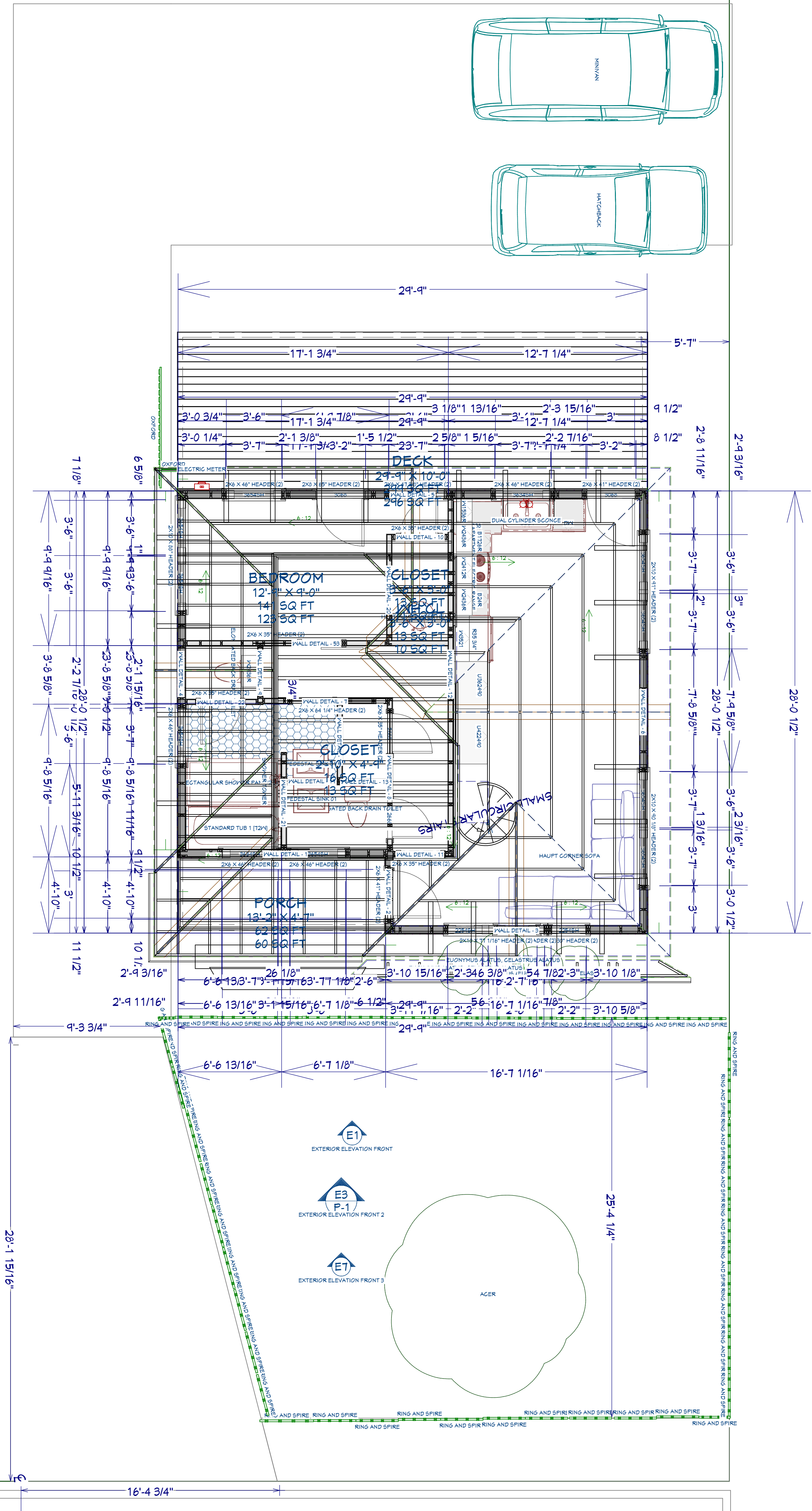
5/3/23

SCALE:

SHEET:

EXTENSION ELEVATION RIGHT 2
E10
P-1
EXTENSION ELEVATION RIGHT

EXTENSION ELEVATION LEFT
E2
EXTENSION ELEVATION LEFT
E4
P-1
EXTENSION ELEVATION LEFT 2
E6
EXTENSION ELEVATION LEFT 3



1st Floor

Floor Plan View Dimensioned

Revisions

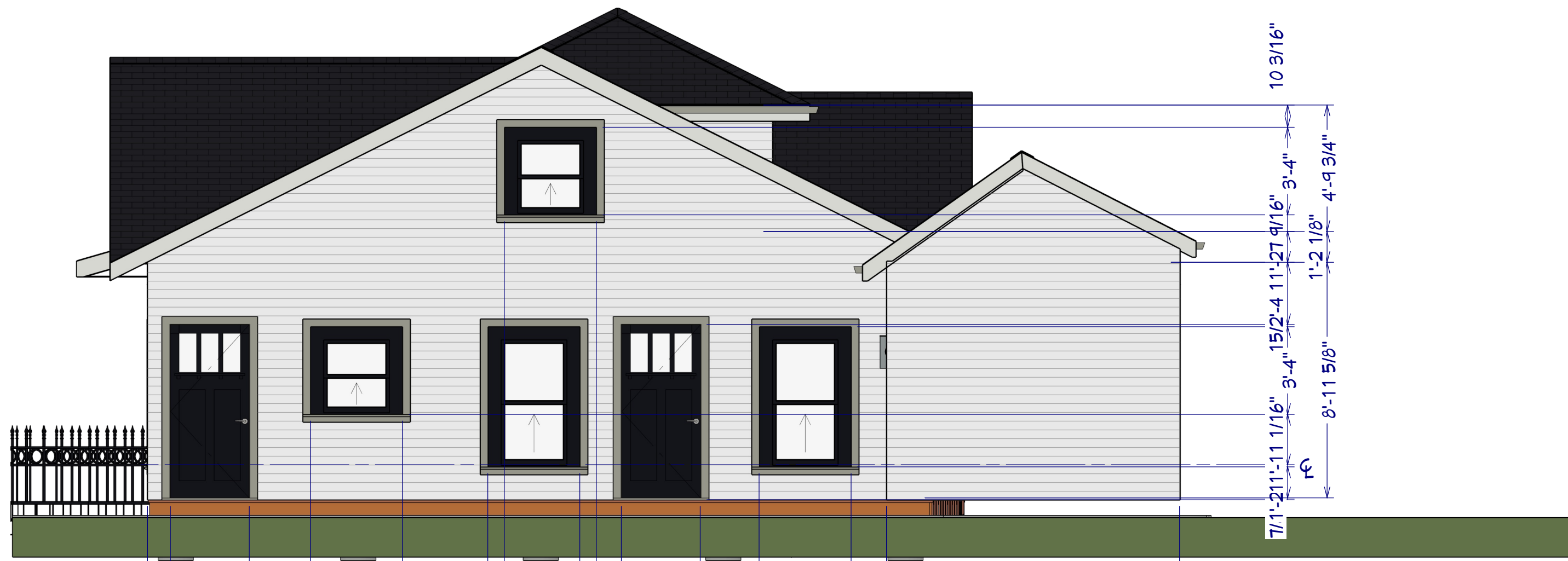
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NUMBER	DATE	REVISED BY	DESCRIPTION

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DATE:
5/9/23

SCALE:

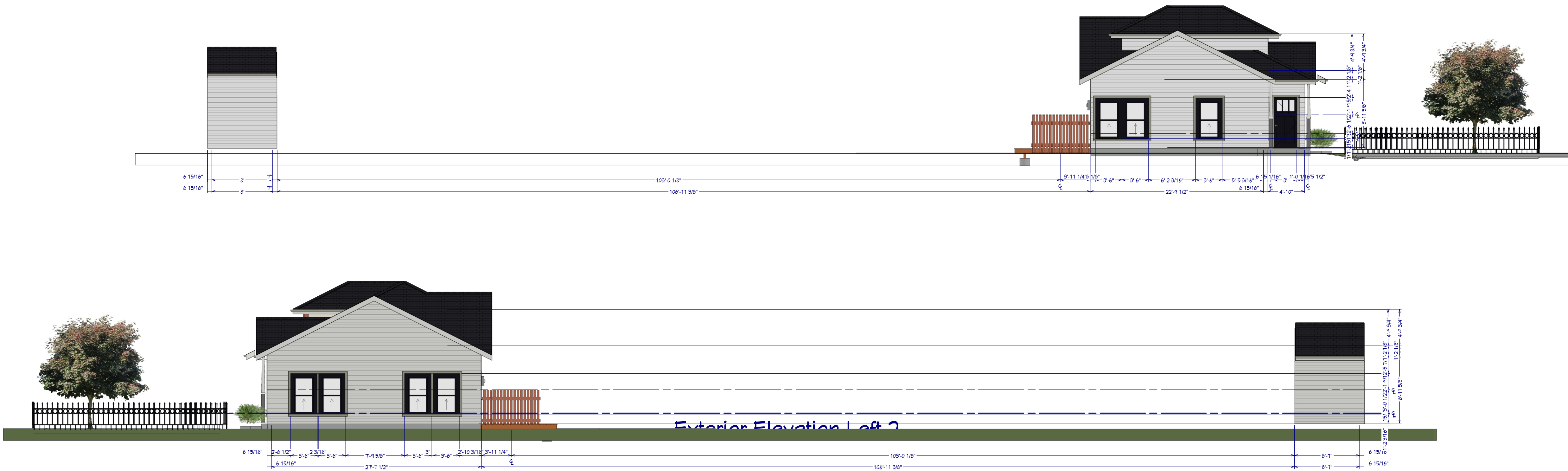
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Exterior Elevation Back 2



Exterior Elevation Front 2



Exterior Elevation Left 2

Exterior Elevation Right 2

REVISION TABLE	
NUMBER	DESCRIPTION

DATE:

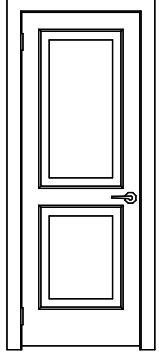
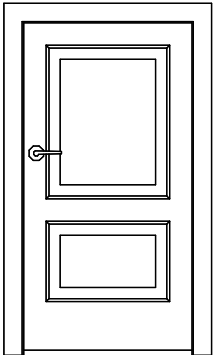
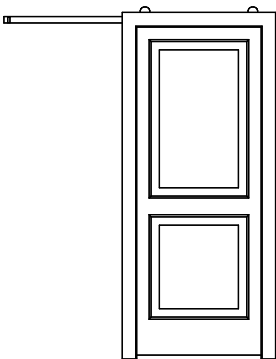
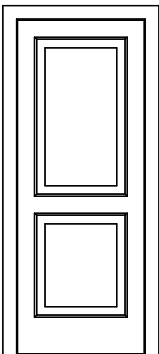
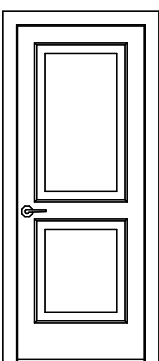
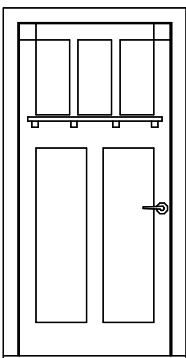
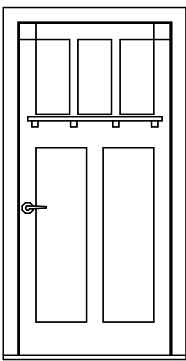
5/8/23

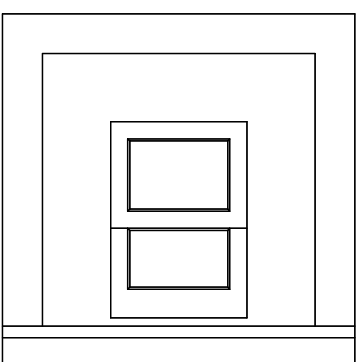
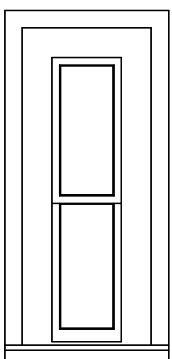
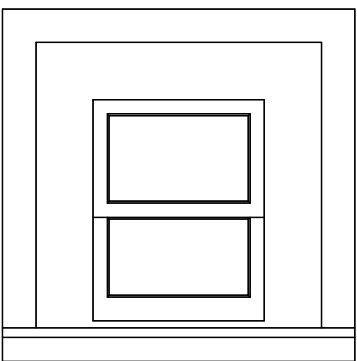
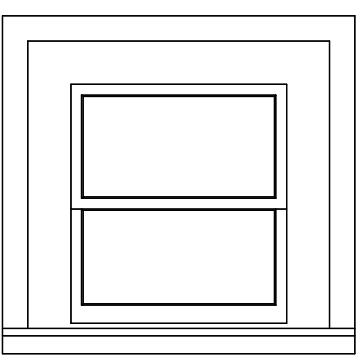
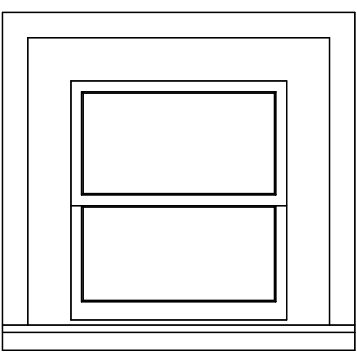
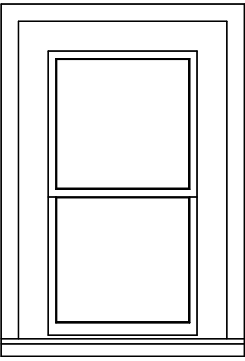
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SHEET:

P-1

Revisions

DOOR SCHEDULE												
3D EXTERIOR ELEVATION	NUMBER	LABEL	QTY	FLOOR	SIZE	R/O	EGRESS	DESCRIPTION	HEADER	CODE	MANUFACTURER	COMMENTS
	D01	2468	1	1	2468 R IN	30"X82 1/2"		HINGED-DOOR P04	2"X6"X33" (2)			
	D02	26411	1	2	26411 R IN	32"X61 3/4"		HINGED-DOOR P04	2"X6"X32" (2)			
	D03	2668	1	1	2668 R	32"X82 1/2"		BARN-DOOR P04	2"X6"X35" (2)			
	D04	2668	1	1	2668 R	61 1/4"X82 1/2"		POCKET-DOOR P04	2"X6"X64 1/4" (2)			
	D05	2668	3	1	2668 R IN	32"X82 1/2"		HINGED-DOOR P04	2"X6"X35" (2)			
	D06	3068	2	1	3068 L EX	38"X83"		EXT. HINGED-DOOR E21	2"X6"X41" (2)			
	D07	3068	2	1	3068 R EX	38"X83"		EXT. HINGED-DOOR E21	2"X6"X41" (2)			

WINDOW SCHEDULE												
3D EXTERIOR ELEVATION	NUMBER	LABEL	QTY	FLOOR	SIZE	R/O	EGRESS	DESCRIPTION	HEADER	CODE	MANUFACTURER	COMMENTS
	W01	2020SH	3	2	2020SH	25"X25"		SINGLE HUNG	2"X6"X28" (2)			
	W02	2254SH	2	1	2254SH	27"X65"		SINGLE HUNG	2"X6"X30" (2)			
	W03	2626SH	1	1	2626SH	31"X31"		SINGLE HUNG	2"X6"X34" (2)			
	W04	3634SH	1	1	3634SH	43"X41"		SINGLE HUNG	2"X6"X46" (2)			
	W05	3634SH	1	2	3634SH	43"X41"		SINGLE HUNG	2"X6"X46" (2)			
	W06	3654SH	12	1	3654SH	43"X65"		SINGLE HUNG	2"X6"X46" (2)			